



The Corporation of the Town of LaSalle

Date:	January 18, 2017	Report No:	DS-05-2017
Directed To:	Mayor and Members of Council	Attachments:	City of Windsor Correspondence and Reports
Department:	Development & Strategic Initiatives		
Prepared By:	L. Silani, M.Pl., MCIP, RPP Director of Planning & Development Services	Policy References:	
	P. Marra, P. Eng Director of Public Works		
Subject:	City of Windsor – January 25, 2017 meeting of their Environment, Transportation & Public Safety Standing Committee --- pertaining to an “Inquiry regarding the process involved for the closure of Matchette Road”		

RECOMMENDATION:

That correspondence be sent to the City of Windsor, advising the City that the Town of LaSalle considers Matchette Road to be an important north-south Collector Road --- and that a comprehensive study of the entire transportation network servicing West/South Windsor and the Town of LaSalle needs to be undertaken before any decisions are made by either municipality to alter the status and planned function of this roadway.

REPORT:

Attached, please find a copy of an email dated January 16, 2017, together with copies of two City of Windsor Staff Reports dated June 16, 2016 and October 19, 2016, informing the Town of LaSalle that the Environment, Transportation & Public Safety Standing Committee of the City of Windsor is meeting on January 25, 2017 --- with one of the items to be discussed being an “Inquiry regarding the process involved for the closure of Matchette Road”.

As Council is aware, the Ontario Municipal Board recently granted approval to Planning Act applications which will permit a major new commercial development to be built in the City of Windsor --- on the northwest corner of Matchette Road and Sprucewood Avenue. Based on information contained within transportation engineering reports that were submitted in support of

this application, Matchette Road will be carrying over 50 percent of all traffic coming to/from this site --- that traffic being from neighbourhoods located throughout the region that would be using the on and off ramps that exist at EC Row Expressway and Matchette Road. These same engineering reports indicate that once this new commercial centre is constructed, the peak hour traffic using Matchette Road will increase to over 800 vehicles per hour in one direction, north of Sprucewood Avenue.

The tri-party agreement that was negotiated between the City of Windsor, the Town of LaSalle and the Coco Group of Companies relies on the fact that this north-south Collector Road will not only remain open, but also that it will be upgraded and improved to accommodate the projected increase in traffic that will be using this roadway to travel to/from this new "regional commercial shopping district".

It is our professional planning and engineering opinion that no alteration to the status and/or function of this important north-south Collector Road be made without first completing a comprehensive analysis of the entire transportation network servicing West/South Windsor and the Town of LaSalle ----- to determine what changes (and corresponding capital improvements) need to be made to the remaining parts of the overall transportation network in order to continue to be able to provide for the safe and convenient movement of existing and planned future traffic for all vehicle types.



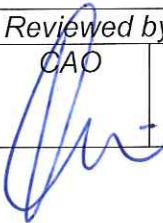
L. Silani, M.Pl., MCIP, RPP
 Director of Development &
 Strategic Initiatives Services



P. Marra, P. Eng
 Director of Public Works

Attachments

<i>Reviewed by:</i>						
<i>CAO</i>	<i>Finance</i>	<i>Clerk</i>	<i>Environmental Services</i>	<i>Development & Strategic Initiatives</i>	<i>Culture & Recreation</i>	<i>Fire</i>



Larry Silani

Subject: January 25, 2017 ETPS Standing Committee meeting
Attachments: jan 25 item 8.7.pdf; jan 25 item 8.6.pdf

From: Toldo, Beth [mailto:toldob@citywindsor.ca]
Sent: January-16-17 3:54 PM
To: Peter Marra <pmarra@lasalle.ca>; 'tbateman@countyofsessex.on.ca' <tbateman@countyofsessex.on.ca>; 'jcoco@cocogroup.com' <jcoco@cocogroup.com>; 'Cathy Greenwell' [REDACTED]; 'nancypancheshan' [REDACTED]; 'lynnm' [REDACTED]
Cc: Kadour, Karen <kkadour@citywindsor.ca>
Subject: January 25, 2017 ETPS Standing Committee meeting

RE: CR248/2014 – Inquiry regarding the process involved for the closure of Matchette Road, Ward 1 (Item 8.7) AND Additional Information Memo regarding CR248/2014 – Inquiry regarding the process involved for the closure of Matchette Road, Ward 1 (Item 8.6)

The attached administrative reports have been scheduled for consideration at the Wednesday January 25, 2017 Environment, Transportation & Public Safety Standing Committee meeting. This meeting will be held in Council Chambers, 3rd floor, Windsor City Hall and will begin at 4:30 p.m. Should you wish to be listed as a delegation and address members of standing committee on this matter, please contact me by noon Friday January 20th. Thanks.

Beth Toldo

Council Agenda Coordinator
Council Services Department, Office of the City Clerk



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Public Works - Operations

MISSION STATEMENT

"Our City is built on relationships – between citizens and their government, businesses and public institutions, city and region – all interconnected, mutually supportive, and focused on the brightest future we can create together"

REPORT #: S 28/2016	Report Date: 06/17/2016
Author's Contact: Jennifer Leitzinger Transportation Planning Engineer 519-255-6247 ext. 6002 jleitzinger@citywindsor.ca	Date to Council: 07/20/2016
	Clerk's File #: AE2016

To: Mayor and Members of City Council

Subject: CR248/2014 – Inquiry Regarding the Process Involved for the Closure of Matchette Rd. – Ward 1

RECOMMENDATION:

THAT report number S28/2016 - CR248/2014 – *Inquiry Regarding the Process Involved for the Closure Matchette Rd.* **BE RECEIVED** for information.

EXECUTIVE SUMMARY:

N/A

BACKGROUND:

Windsor City Council adopted the following resolution at its meeting held on September 9, 2014:

CR248/2014

*That Administration **BE DIRECTED** to prepare a report for Council's consideration outlining the process to close Matchette Road, including information on the process for an environmental assessment and options on how such a study could be funded.*

Matchette Road is a Class 1 Collector within the City of Windsor jurisdiction. A full interchange with the Rt. Hon. Herb Gray Parkway has been recently constructed. As of

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2015, the Annual Average Daily Traffic (AADT) for Matchette Road in the vicinity of Sprucewood Ave. is 9,800 vehicles per day (vpd). The road is currently a rural cross section with gravel shoulders and open drainage ditches. The pavement width is 7.3m. A section of Matchette Road lies between Ojibway Park and the Ojibway Prairie Provincial Nature Reserve which are part of the Ojibway Prairie Complex.

The City Limits fall within the intersection of Sprucewood Ave. and Matchette Road. Sprucewood Ave within the City Limits is a Class II Collector. Within the Town of LaSalle, Sprucewood Ave. carries the Regional Road designation of Connecting Link.

The Bicycle Use Master Plan (BUMP) recommends cycling facilities on Matchette Road from Prince Road to the City Limits and connecting into LaSalle.

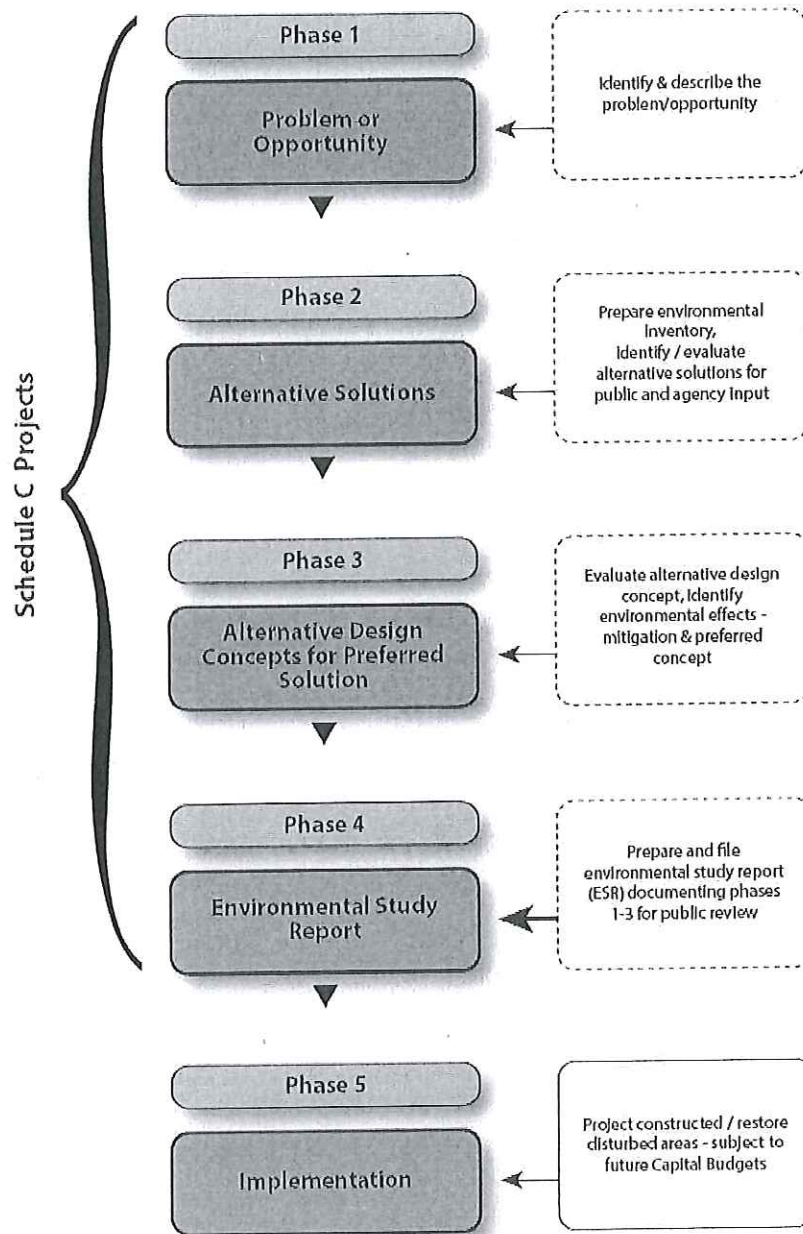
DISCUSSION:

The Municipal Class Environmental Assessment (MCEA) (October 2000, as amended in 2007, 2011 & 2015) applies to municipal projects including roads, water and wastewater projects, and transit. Since projects undertaken by municipalities can vary in their environmental impact, such projects are classified in this Class EA in terms of schedules being Schedule A+ for projects with minor impacts to Schedule C with a higher potential for more significant impacts. The closure of Matchette Road would require a Schedule C process.

The reasons for a Schedule C EA process are related to the nature of the potential impacts of a road closure along Matchette and satisfying the need to consult with the potentially impacted parties. These include the following:

- Impact on adjacent road networks of existing 9,800vpd redirected from Matchette Road
- Potential elimination of access to existing developed lands and lands to be developed in the future
- Interconnection with the Rt. Hon. Herb Gray Parkway and the Gordie Howe International Bridge
- Recent OMB settlement with Town of LaSalle for intersection improvements at Matchette and Sprucewood, and
- Requirement to consult with parties potentially affected by the road closure such as the abutting property owners, the public in both Windsor and LaSalle, various governments such as the Town of LaSalle, County of Essex, the provincial and federal governments, municipal departments and other agencies.

The following flowchart outlines the four phases of a Schedule C MCEA process that lead to the 5th phase Implementation:



RISK ANALYSIS:

No associated risks have been identified with this report.

FINANCIAL MATTERS:

A Municipal Class EA Schedule C study is estimated to range in cost between \$150,000 and \$250,000 depending on the complexity of the scope identified. Funds for this study have not previously been approved in the City Engineer's budget. If Council wishes to proceed with this study, a reallocation of funds in the 2017 capital budget would be required to provide the necessary funding.

CONSULTATIONS:

Parks Department

Planning Department

Legal Department




CONCLUSION:

A MCEA Study would identify alternative solutions to address the problem or opportunity and establish the preferred solution taking into account public and review agency input.

Should Council choose to pursue a study undertaking, it is recommended that the matter be referred to the 2017 budget deliberations.

NOTIFICATIONS:

Name	Address	Email
Town of LaSalle Peter Marra, Director of Public Works	The Corporation of the Town of LaSalle 5950 Malden Road LaSalle, Ontario N9H 1S4	
County of Essex Tom Bateman, County Engineer	County of Essex Administration Offices Essex County Civic Centre 360 Fairview Avenue West	

Name	Address	Email
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Jenny Coco Chief Executive Officer	Coco Paving Inc. 485 Little Baseline Windsor, ON N8N 2L9	
Cathy Greenwell		
Nancy Panchesan		
Lynn Meloche		

APPENDICES:



Public Works - Operations

MISSION STATEMENT

"Our City is built on relationships – between citizens and their government, businesses and public institutions, city and region – all interconnected, mutually supportive, and focused on the brightest future we can create together"

REPORT #: S 187/2016	Report Date: 10/19/2016
Author's Contact: Jennifer Leitzinger Transportation Planning Engineer 519-255-6247 ext. 6002 jleitzinger@citywindsor.ca	Date to Council: 11/23/2016
	Clerk's File #: ZB/7801 ZO/7802

To: Mayor and Members of City Council

Subject: Additional Information Memo - CR248/2014 - Inquiry Regarding the Process Involved for the Closure of Matchette Road - Ward 1

RECOMMENDATION:

THAT the Additional Information Memo pertaining to S28/2016 **BE RECEIVED** for information.

EXECUTIVE SUMMARY:

N/A

BACKGROUND:

Administration Report S28/2016 ("CR248/2014 – Inquiry Regarding the Process Involved for the Closure of Matchette Road") was brought before the Environment, Transportation & Public Safety Standing Committee on July 20, 2016 and the Standing Committee rendered the following decision:

THAT the report of the City Engineer entitled "CR248/2014 - Process Involved for the Closure of Matchette Rd." BE DEFERRED to a future meeting of the Environment, Transportation & Public Safety Standing Committee to allow for administration to report back on options, including costs for the feasibility of wildlife crossing measures for Matchette Road.

DISCUSSION:

The option to close Matchette Road was detailed in the basis report S28/2016 and as per the original request, identified the process involved for the undertaking.

There are five (5) wildlife crossing options detailed in this report that can be grouped into 2 categories:

1. Vehicle over/under pass with wildlife at existing grade; and
2. Wildlife over/under pass with vehicles at existing grade.

1A. Bridge – Vehicles Above Grade

This option connects habitat and natural areas by allowing wildlife movement to occur at-grade under roadways. A vehicle bridge on Matchette Road would support natural overland hydrology and allow vegetation to re-establish in natural succession along the former roadway.

Some design considerations include sufficient bridge under-height to support large mammal travel, opportunities to minimize concentration of wildlife at entrances, features to prohibit wildlife access to the bridge which may include fencing, and design features to minimize on-going maintenance.

1B. Tunnel-Vehicles Below Grade

This option involves tunneling the vehicles below grade level allowing wildlife to cross at grade which in turn supports natural corridors. Vegetation will re-establish in natural succession.

Some design considerations include tunnel drainage, features to prohibit wildlife access to the tunnel and minimize on-going maintenance.

Tunnel Top 5 crossing on the Rt. Hon. Herb Gray Parkway, located near Oakwood is a recent example of this type of wildlife crossing, as shown in **Figure 1**.

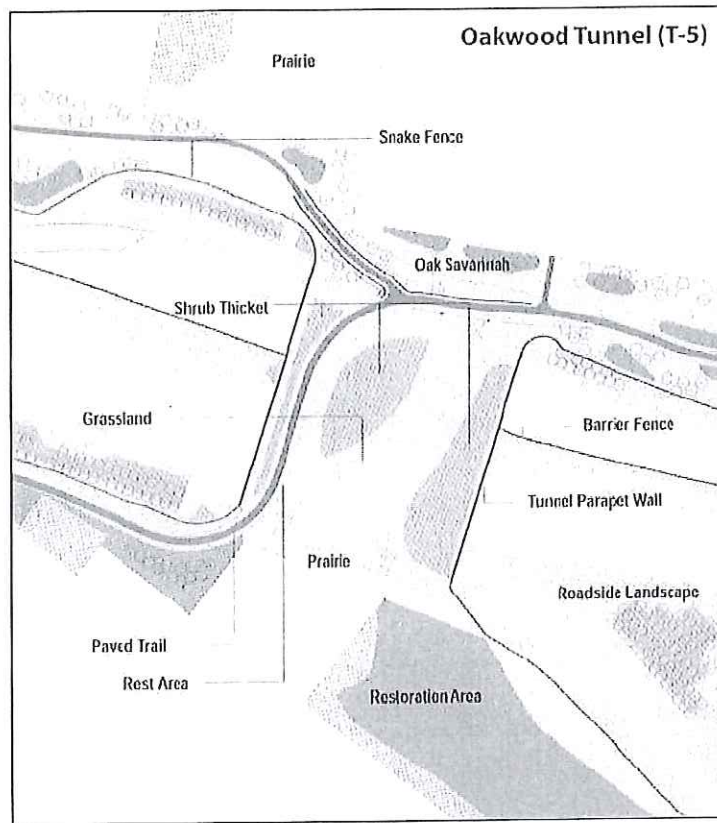


Figure 1: Oakwood Tunnel (T-5) (Source: www.hgparkway.ca)

2A. Bridge - Landscape Above Grade

This design includes a wildlife and landscape bridge over the existing roadway. This design supports the natural overland hydrology and the establishment of new movement patterns across the existing roadway.

Design considerations include sizing the bridge to support a diversity of wildlife use, including amphibians, reptiles, and large mammals such as deer and opportunities to minimize concentration of wildlife at entrances. Design features should be considered to minimize on-going maintenance for wildlife exclusion fencing and habitat creation on the bridge.

A recent example of this type of crossing is on Highway 69 near the Highway 637 (Killarney Road) junction south of Sudbury, as seen in **Figure 2**. Wildlife mitigation measures along Highway 69 included a 30m wide wildlife bridge. (Source: Reducing Wildlife Collisions: What is Working in Northeastern Ontario, Andrew Healy, Environmental Planner, MTO and Kari E. Gunson, Road Ecologist, Eco-Kare International) This overpass was designed specifically to accommodate elk, deer, moose and bear.

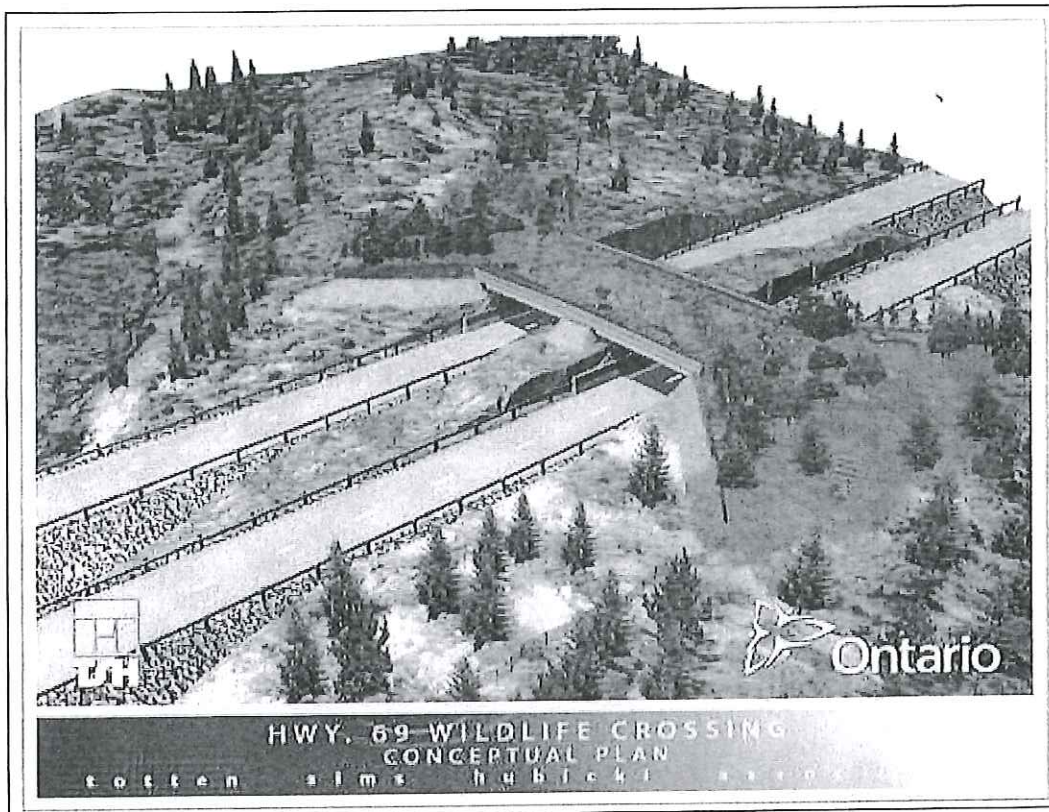


Figure 2: Wildlife Overpass on Highway 69
 (Provides a movement corridor for wildlife to safely cross this section of Ontario highway)

2B. Tunnel Landscape Below Grade

This option involves the creation of a wildlife and landscape tunnel below the existing roadway. This design supports the establishment of new movement patterns across the existing roadway.

Design considerations include the provision of sufficient width and vertical clearance to accommodate the passage of a variety of wildlife, including large mammals as well as opportunities to minimize concentration of wildlife at entrances. Design features will consider tunnel drainage and sufficient sunlight to penetrate the tunnel to support use by wildlife and plant establishment. The installation and on-going maintenance of wildlife exclusion fencing is another consideration

An example can be seen in **Figure 3**.



Figure 3: An Example of Tunneling Landscape Below Grade

References related to the information discussed in the report are available in **Appendix 'A'**.

2C. – Wildlife Eco Passage for Small Animals

An eco passage for small animals attracts certain species and would be considered a mitigation measure. An example of this is being installed in conjunction with the Rt. Hon. Heb Gray Parkway on Matchette Road near the Parkway crossing.

While there are varying design considerations for each option, with the exception of 2C (Wildlife Eco Passage for Small Animals), all wildlife crossing concepts explored connect the two park areas and separate the vehicle traffic from the wildlife. This also improves safety for roadway users and wildlife and consequently separates the vehicles from park users.

RISK ANALYSIS:

As stated in report S28/2016 - CR248/2014 – Inquiry Regarding the Process Involved for the Closure of Matchette Rd. no associated risks have been identified with this report.

FINANCIAL MATTERS:

Partnerships and Funding

Partnering with other organizations will not only demonstrate the wide support for this project but it will also provide project assistance and additional funding opportunities which may not be available to our municipality.

A variety of funding sources are available for this type of connectivity and linkage project. Specifically those related to habitat creation, connectivity and linkage, species at risk recovery and climate change preparedness. Possible funding sources include:

a) EcoAction Community Funding Program

<http://www.ec.gc.ca/ecoaction/>

The program is action focused and encourages projects that enhance or rehabilitate the natural environment and increase the ability of communities to sustain these activities into the future. Non-profit groups and organizations are eligible to receive funding. For every dollar received from the EcoAction Community Funding Program, you must receive at least the same amount from non-federal government partners. This includes cash contributions and in-kind support. The maximum amount of funding that may be provided is \$100,000. The maximum length of a funded project is 36 consecutive months.

b) Canadian Wildlife Federation

<http://cwf-fcf.org/en/foundation/foundation/foundation-funding-programs.html?src=menu>

Up to \$10,000 is given to projects that mitigate damage and enhance and integrate habitat and development.

c) Wildlife Habitat Canada

<http://whc.org/>

Wildlife Habitat Canada (WHC) has provided over \$50 million in grants to more than 1,500 habitat conservation projects across Canada.

d) Ontario Trillium Foundation (OTF)

<http://www.otf.ca/>

In the section of the OTF entitled "Green People" more than \$50,000 has been given to individual projects that support sustainable conservation and restoration efforts and have a positive impact on the lives of people in the community.

Appendix 'B' provides more details on funding opportunities.

The following are **very high level** cost estimates related to the feasibility of wildlife crossing measures for Matchette Road.

1A and 1B - Overpass/Underpass Roadway

Costs are estimated in the range of \$10 million to \$20 million as it is dependent on the scope (length, height, soil conditions (*this area is known to have very poor soil conditions*), and overall complexities.

2A - Landscape Bridge

This option can cost in the range of \$5 million to \$20 million depending on scope (length, height, soil conditions *(this area is known to have very poor soil conditions)*, and overall complexities.

2B - Tunnel Landscaping Below Grade

This option can cost in the range of \$3 million to \$10 million dependent on the scope (length, depth, soil conditions *(this area is known to have very poor soil conditions)*, and overall complexities.

2C - Small Eco Passage for Snakes or Small Animals

Similar to the Ministry of Transportation (MTO) eco-passage on Matchette Road near Chappus Street is estimated at \$150,000 to \$200,000.

CONSULTATIONS:

Parks Department
Development, Projects & ROW Department
Office of the City Solicitor
Office of the City Treasurer

CONCLUSION:

The additional information is provided further to Council direction.

PLANNING ACT MATTERS:

N/A

APPROVALS:

Name	Title
Josette Eugeni	Manager of Transportation Planning
Cindy Becker	Financial Planning Administrator
Dwayne Dawson	Executive Director of Operations
Mark Winterton	City Engineer
Shelby Askin Hager	City Solicitor
Joe Mancina	City Treasurer

Name	Title
Onorio Colucci	Chief Administrative Officer

NOTIFICATIONS:

Name	Address	Email
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County of Essex Tom Bateman, County Engineer	County of Essex Administrative Offices Essex County Civic Centres 360 Fairview Avenue West Essex, Ontario N8M 1Y6	tbateman@countyofessex.on.ca
Jenny Coco Chief Executive Officer	Coco Paving Inc. 485 Little Baseline Windsor, ON N8N 2L9	jcoco@cocogroup.com
Cathy Greenwell		[REDACTED]
Nancy Panchesan		[REDACTED]
Lynn Meloche		[REDACTED]

APPENDICES:

Appendix A - References

Appendix B – Funding Opportunities

Appendix 'A'

References

JD Choquette, L Valliant. Road Mortality of Reptiles and Other Wildlife at the Ojibway Prairie Complex and Greater Park Ecosystem in Southern Ontario- The Canadian Field-Naturalist, 2016

Silvy, Nova J., ed. The Wildlife Techniques Manual. Volume 1: Research. 7th ed. Vol. 1. Baltimore, MD: Johns Hopkins UP, 2012. Print.

Silvy, Nova J. The Wildlife Techniques Manual. Volume 2: Management. 7th ed. Vol. 2. Baltimore, MD: Johns Hopkins UP, 2012. Print.

Ontario Road Ecology Group, Toronto Zoo. 2010., and Environment Canada Habitat Stewardship Program For Species At Risk. "A Guide to Road Ecology in Ontario." (2010): n. pag. Web. <https://www.rom.on.ca/sites/default/files/imce/oreg_final.pdf>.

U.S. Department of Transportation, and Federal Highway Administration. "WILDLIFE CROSSING STRUCTURE HANDBOOK." N.p., Mar. 2011. Web.

Planning & Building Services Department. "Windsor - Official Plan." City of Windsor. N.p., n.d. Web. 24 Sept. 2016. <<http://www.citywindsor.ca/residents/planning/Plans-and-Community-Information/Windsor---Official-Plan/Pages/Windsor-Official-Plan.aspx>>.

City of Windsor, and Parks And Recreation. "City of Windsor Parks & Recreation Master Plan." Parks Master Plan. N.p., n.d. Web. 20 Sept. 2016. <<http://www.citywindsor.ca/residents/parksandforestry/Parks-Development/Pages/Parks-Master-Plan.aspx>>.

Ministry of Municipal Affairs and Housing. "Provincial Policy Statement, 2014." Provincial Policy Statement, 2014. N.p., n.d. Web. 20 Sept. 2016. <<http://www.mah.gov.on.ca/Page10679.aspx>>.

Herb Gray Parkway. Lura Consulting 2016, n.d. Web. 26 Sept. 2016. "Design and Construction Report #8." <<http://www.hgparkway.ca/info-centre>>.

"LPCIP Introduction and Overview." Long Point Causeway Improvement Project. Long Point World Biosphere Reserve Foundation, Apr. 2008. Web. 26 Sept. 2016. <<http://longpointcauseway.com/introduction-and-overview/>>.

Ecopassage Funding Opportunities

Provincial

Ontario Species at Risk Stewardship Fund (SARSF)

<https://www.ontario.ca/page/species-risk-stewardship-fund-application-guidelines>

The Species at Risk Stewardship Fund was created under the Endangered Species Act, and targets species listed as extirpated, endangered, threatened, or special concern on the Species at Risk in Ontario List. The natural areas on either side of Matchette Road are home to several species at risk, including Blanding's turtle and Eastern Foxsnake.

Open to municipalities. Deadline has passed to apply for 2017/2018 projects, but new application deadlines will be added soon.

The Longpoint Causeway project received approximately \$440,000 from this source over a 5 year period.

Land Stewardship and Habitat Restoration Program

<https://www.ontario.ca/page/land-stewardship-and-habitat-restoration-program>

Up to \$20,000 in matching funds for a project that maintains or restores habitats that benefit fish, animals and/or plants.

Municipalities are eligible. Even if Ecopassage construction itself is not eligible, habitat restoration and mitigation measures as part of the development will be.

Ontario Trillium Foundation

<http://otf.ca/what-we-fund/investment-streams/capital-grants>

http://otf.ca/sites/default/files/capital_stream.pdf

The amount of funding ranges from \$5000-\$150,000 depending on the project. The full amount of funds granted by the OTF will not be provided right away, 10% of the grant money will be held back until the project is complete and is found to be satisfactory. The type of OTF grant that this funding would be coming from would be classified as a capital grant, which provides funding for 1 year. There are also other OTF grant options to be considered, such as the Collective Impact Grant, which depends on funding partnership. Multiple aspects of this project are similar to examples of previously funded costs.

- Renovations or repairs to community spaces
- Outdoor installations
- Purchase of land or building

- Developmental costs associated with construction, such as the development of plans, legal fees and/or survey costs that are part of a capital grant; these costs are limited to 20% of the total grant
- Project-related general contractor costs
- Equipment costs (including technology)

Federal

Habitat Stewardship Program for Species at Risk

<http://www.ec.gc.ca/hsp-pih/default.asp?lang=En&n=59BF488F-1>

Ontario

SAR Stream: (416) 739-4986, Prevention Stream: (416) 739-4100

This federal government program provides funding for projects that conserve and protect species at risk and their habitats.

Approximately \$12.2 million allotted for projects that fit criteria, including activities which take place in aquatic areas across Canada. This development would qualify as the Blanding's Turtle, which has recently been declared endangered and added to the International Union for Conservation of Nature and Natural Resources Red List, is a regular roadkill casualty that would greatly benefit from safe ecopassage development.

The program requires a minimum of 1:1 leveraging on funds invested - for every \$1 provided by the HSP, at least \$1 is raised by project recipients. This leveraging can be either financial or in-kind resources (equipment loans, donations of building materials and volunteer labour). Partner funding and other support broaden the scope of projects, improve on-the-ground results, and strengthen the public and private collaboration that is essential to involving all Canadians in stewardship activities for all species.

The Longpoint causeway project received approximately \$223,000 from this program over a 3 year period.

Great Lakes Sustainability Fund

<https://www.ec.gc.ca/raps-pas/default.asp?lang=En&n=F328E319-1>

Provides technical and financial support to projects (up to one-third of the total cost). The project must implement remedial actions to complete the clean up or restoration in three key priority areas: fish and wildlife habitat rehabilitation and stewardship, contaminated sediment assessment and remediation, and innovative approaches to improve municipal wastewater effluent quality. The project that is being proposed falls under the category of fish and wildlife habitat rehabilitation and stewardship.

The Canadian Wildlife Foundation

<http://cwf-fcf.org/en/foundation/foundation/foundation-funding-programs.html?src=menu>

Grants typically range in value from \$500 to \$5000. Financial assistance will be allocated for a maximum of one year, though renewals will be considered.

Applications for funding assistance will be considered from registered Canadian charitable and non-profit organizations, such as the Friends of Ojibway Prairie, Tallgrass Ontario, and the Essex County Field Naturalists Club.

National Wetland Conservation Fund (NWCF)

<https://www.ec.gc.ca/financement-funding/default.asp?lang=En&n=56914323-1>

Municipalities are eligible. Fund matching in a 1:1 ratio, up to a maximum of 50% of total cost. Eligibility based on enhancement of habitat for aquatic species at risk, including the Blanding's and stinkpot turtle under section 2 - the manipulation of the physical and/or biological characteristics of a habitat site, usually to address human caused impacts, to improve the ecological functioning of the degraded wetland habitat. The result may produce no gain in habitat area but produces a gain in wetland function.

\$120,000 received by the Longpoint Causeway project in 2014.